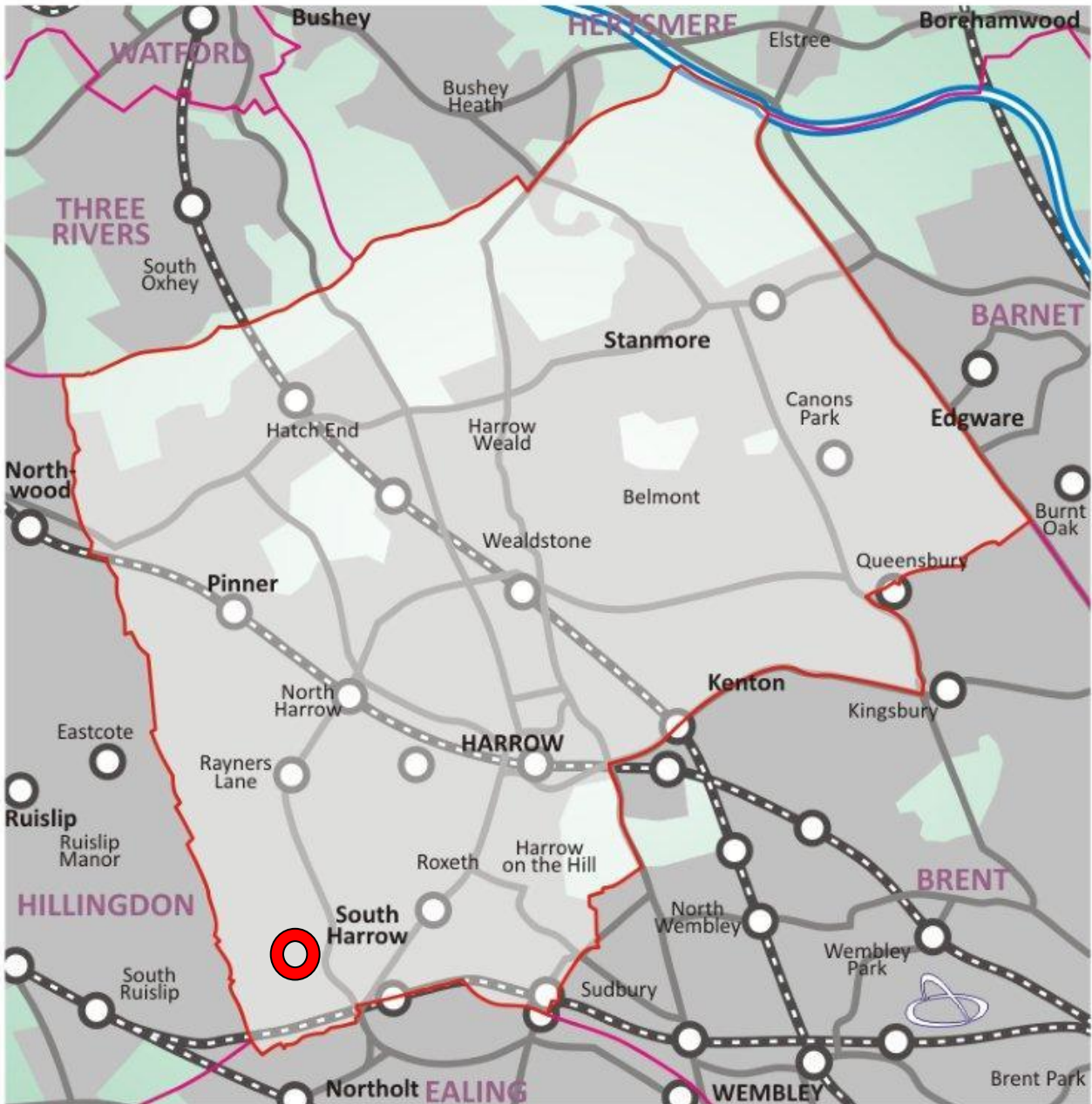


 = application site

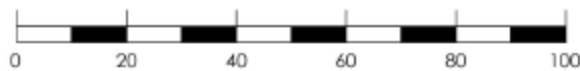


**Rooks Heath College, Eastcote Lane, South Harrow, HA2 P/1907/22
9AH**

Location Plan



Site Location Plan @ 1:1250



Scale in metres

LONDON BOROUGH OF HARROW

PLANNING COMMITTEE

19th April 2023

APPLICATION NUMBER: P/1907/22
VALID DATE: 24TH MAY 2022
LOCATION: ROOKS HEATH COLLEGE, EASTCOTE LANE
WARD: ROXBOURNE
POSTCODE: HA2 9AH
APPLICANT: MR SCOTT COOK
AGENT: ACADEMY ESTATE CONSULTANTS
CASE OFFICER: AKSHAY SISODIA
EXPIRY DATE: 24/04/2023 (EOT)

PROPOSAL

Redevelopment to provide two storey teaching block (Use class F1a) (demolition of existing teaching block), associated alterations to staff car parking, provision of cycle parking.

RECOMMENDATION

The Planning Committee is asked to:

- 1) Agree the reasons for approval as set out in this report, and
- 2) Grant planning permission subject to the Conditions listed in Appendix 1 of this report.

REASON FOR THE RECOMMENDATION

The proposal would result in an improvement in the school's educational facilities whilst ensuring that the proposed building would not detract from the character and appearance of the application site, street scene and area in a wider context, and would have an acceptable impact upon the residential amenity of neighbouring properties. The replacement building would be appropriately accessible for both able-bodied and disabled users. The development's provision of car and cycle parking is acceptable, and compliance with the submitted travel plan ensures that there would be sustainable travel by students, staff and visitors to the school. The development is not considered to be susceptible to harmful flooding and is not considered to exacerbate flood risk elsewhere. Insufficient detail has been provided on the development's compliance with Secured by Design and fire safety requirements, additional detail on this can be requested and secured via conditions, a condition has also been applied requiring the applicant to submit a Construction Logistics Plan given the scale of the development and demolition works required. The Council's Transport Officer has requested Swept Path Analysis drawings to demonstrate that proposed replacement car parking spaces can be safely accessed, this has been requested and will be provided as part of an addendum. In light of the above, subject to conditions and the provision of swept path drawings, the proposed development would be in accordance with the National Planning Policy Framework (2021), Policies GG1, GG2, GG3, D3, D4, D5,

D11, D12, D13, D14, S1, S3, SI 12, SI 13, T1, T2, T3, T4, T5, T6, T7 of the London Plan (2021), Policies CS1.B, CS1.E, CS1.T, CS1.U, CS1.W, CS1.Z, of the Harrow Core Strategy, Policies DM1, DM2, DM9, DM10, DM12, DM14, DM22, DM42, DM43, DM44, DM46 of the Harrow Development Management Policies Local Plan.

INFORMATION

This application is reported to Planning Committee as it relates to development on land owned by the Council and therefore falls within proviso C (i) of the Scheme of Delegation.

Statutory Return Type:	(E)18.Minor Development, all other
Council Interest:	Council is freeholder
Net additional Floorspace:	Approximately 796m ²
GLA Community Infrastructure Levy (CIL) Contribution (provisional):	N/A
Local CIL requirement:	N/A

HUMAN RIGHTS ACT

The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report.

EQUALITIES

In determining this planning application the Council has regard to its equalities obligations including its obligations under section 149 of the Equality Act 2010.

For the purposes of this application there are no adverse equalities issues.

S17 CRIME & DISORDER ACT

The applicant has failed to provide any detail on the proposed development's resilience to crime. For the avoidance of doubt, a condition has been imposed requiring the applicant to submit evidence of certification of Secure by Design accreditation prior to the proposed building being occupied.

1.0 SITE DESCRIPTION

- 1.1 The application relates to Rooks Heath School and Sixth Form located towards the north eastern side of Eastcote Lane. The school is a public mixed-gender secondary school and provides education to children between the ages of 11-18.
- 1.2 The site is comprised of a number of teaching buildings (ranging from 1 to 2 storeys in height - the main school building has usable loft space with this appearing 3 storeys in height) and has car parking to the front and to the rear of the site. There are two hardsurfaced play spaces to the rear of the site.
- 1.3 To the west, the application site is flanked by a parade of commercial properties along Eastcote Lane with residential units above, residential properties along Bodmin Close are to the rear of this, and residential properties along Hamilton Crescent are further to the rear.
- 1.4 To the east the application site is flanked by residential properties along Eastcote Lane and Heathland Primary School further rear of this.
- 1.5 There are numerous residential properties immediately to the north of the site along both Merlins Avenue and Minehead Road.
- 1.6 The main access to the site is via Eastcote Lane, however there is a secondary access to the rear of the site off Merlins Avenue. Both entrances are gated.
- 1.7 There are no statutory or locally listed buildings within the confines of the site, and the site is not located within a Conservation Area.
- 1.8 The application site located within a Critical Drainage Area, and sits partly within Flood Zones 3a and 3b, however this only applies to small patches of land to the front and very rear of the site.

2.0 PROPOSAL

- 2.1 The removal of a modular portacabin style two storey 9 classroom block with associated offices, WCs and circulation space which is sited to the west of the main school building. The installation of a replacement two storey teaching building accommodating 8.no class rooms, associated offices, WCs, a plant room and circulation space, to rear of the site within the area currently allocated to car parking. The building would be served by two stair cores and a lift. The building is to be of masonry construction with facing brickwork and render finishes, and it would incorporate a flat roof form with a slight variation in height.
- 2.2 The development would result in the loss of 20 car parking spaces within the rear car park, 19 existing parking spaces would remain to the rear of the site and a further 2.no car parking spaces are to be created to the rear of the site (totalling 21 to the rear). A total of 16 replacement car parking spaces are to be provided beside the area that is to be created following the demolition of the existing building, meaning that the development would result in a net loss of 2.no car parking spaces. 20 temporary car parking spaces are to be provided within the external playing cage to

the north western corner of the site during the site works, the playing space within the area is to be reinstated once the existing surplus building is demolished. There are 37 car parking spaces to the front of the site that would remain, and there are a further 14 spaces within a car park that the school shares with Heathland School.

2.3 New sheltered and secure cycle parking is to be provided as part of the development. This consists of; 20 visitor/staff cycle parking spaces to the front of the site (with a 50/50 split), 2x 40 student cycle parking spaces within the area of the demolished building 20 staff cycle parking spaces further north of this, and a further 40 student cycle parking spaces slightly to the north east of this. There would be a total of 120 student cycle parking spaces, 30 staff cycle parking spaces, and 10 visitor cycle parking spaces with a grand total of 160 proposed cycle parking spaces.

2.4 The school is proposing to put in place initiatives to reduce single occupancy car use to and from the school and encourage more sustainable modes of transport. These initiatives are outlined within the submitted School Travel Plan. This Travel Plan is not solely linked to the planning application, it will be an on-going element of the operation of the school going forward.

2.5 REVISIONS TO THIS APPLICATION

2.6 The following revisions have been made to this application:

- The applicant was not initially proposing to provide replacement car parking and proposed cycle parking. The scheme was amended to provide replacement car parking to avoid adverse impacts relating to overspill parking to surrounding roads. Cycle parking was provided in order to allow for more sustainable travel for students, staff and visitors.
- The applicant has provided a Parking Assessment
- The Council has provided a School Travel Plan
- Details on proposed cycle parking shelters were provided
- The application description was altered to reference changes to car parking arrangements and the provision of cycle parking.

3.0 **RELEVANT PLANNING HISTORY**

3.1 A summary of the planning application history is set out below:

Ref no.	Description	Status & date of decision
HAR/5672	ERECT LAVATORY ACCOMMODATION	Granted 13/11/1951
HAR/5672/A	CONTINUED USE LAVATORY ACCOMMODATION	Withdrawn 01/01/1965
WEST/45836/92/FUL	TWO STOREY BLOCK TO PROVIDE CLASSROOM ACCOMMODATION WITH COVERED LINK	Granted 01/02/1993
WEST/44568/92/FUL	APPLICATION UNDER REG.4 T & C PLANNING GENERAL REGS.1976 :INFILL EXTENSION & ALTERATIONS TO GROUND FLOOR BLOCK B	Granted 26/05/1992
WEST/42/93/FUL	LAYING OUT OF 3 TENNIS COURTS WITH 3.6M HIGH BALL SCREEN FENCING AND RELOCATION OF 17 CAR PARKING SPACES	Granted 24/05/1993
WEST/26/94/LA3	NEW FIRE ESCAPE AND TWO GLAZED WINDOW BOXES ON LABORATORY BUILDING	Granted 19/04/1994
WEST/482/94/LA3	ENCLOSURE OF TWO COVERED WALKWAYS, NEW WALL AND RAILINGS AND DISABLED ACCESS FACILITIES.	Granted 10/10/1994
WEST/430/98/LA3	PROVISION OF FIRE ESCAPE	Granted 09/09/1998
WEST/50/00/FUL	SINGLE STOREY REAR EXTENSION TO PROVIDE TOILET FACILITIES.	Granted 08/03/2000
P/1930/03/DFU	ENTRANCE PORCH AND REPLACEMENT GATES.	Granted 17/11/2003
P/1921/04/DLA	CHANGE OF USE: CARETAKER'S DWELLING (CLASS C3) TO POLICE OFFICE (CLASS B1) & ALTERATIONS TO	Granted 29/09/2004

	ELEVATIONS INCLUDING AIR CONDITIONING UNITS AND EXTRACT FLUES	
P/2545/04/DCO	RETENTION OF ALTERATIONS TO ELEVATIONS INCLUDING AIR CONDITIONING UNITS AND EXTRACT FLUES	Granted 14/12/2004
P/1589/05/CLA	PART SINGLE/PART TWO STOREY BUILDING WITH GLAZED LINK AT EASTCOTE LANE FRONTAGE OF SITE	Granted: 16/08/2005
P/4052/07	SINGLE AND TWO-STOREY EXTENSION FRONTING EASTCOTE LANE AND EXTERNAL ALTERATIONS TO PROVIDE POST 16 (SIXTH FORM) EDUCATIONAL FACILITIES	Granted 28/01/2008
P/1678/08DFU	SINGLE STOREY UNIT ADJACENT TO FRONT SITE BOUNDARY FOR TEMPORARY TWO-YEAR PERIOD AND TWO-STOREY UNIT ADJACENT TO EASTERLY SITE BOUNDARY FOR TEMPORARY FIVE-YEAR PERIOD	Granted 24/07/2008
P/0385/09	DETAILS OF MATERIALS PURSUANT TO CONDITION 2 OF P/4052/07/DFU RELATING TO A SINGLE AND TWO STOREY EXTENSION FRONTING EASTCOTE LANE AND EXTERNAL ALTERATIONS TO PROVIDE SIXTH FORM EDUCATIONAL FACILITIES GRANTED 28.01.2008	Approved 17/04/2009
P/1287/09	TWO STOREY INFILL EXTENSION (FIRST FLOOR AND MEZZANINE) TO PROVIDE NEW THREE STOREY LIFT SHAFT AND REPLACEMENT OF WINDOWS TO REAR (EAST) ELEVATION	Granted 09/09/2009
P/2158/09	TWO TEMPORARY SINGLE STOREY MODULAR BUILDINGS (TOTAL INTERNAL FLOOR AREA OF 297 CUBIC METRES)	Granted 06/11/2009
P/2489/09	NEW CANOPY TO MAIN ENTRANCE ON FRONT ELEVATION; EXTERNAL	Granted 16/03/2010

	ALTERATIONS; REPLACEMENT AND NEW 2M HIGH SECURITY FENCING WITH GATES ALONG ACCESSWAY, CAR PARK, PLAY AREAS AND SITE BOUNDARY; SOFT LANDSCAPING; WIDENING OF VEHICLE ACCESS TO EASTCOTE LANE AND ALTERATIONS TO PEDESTRIAN ENTRANCE; RAISED FOOTPATH AND HARDSURFACING; ILLUMINATED BOLLARDS AND LIGHTING; MODIFICATIONS TO CAR PARK LAYOUT; NEW REFUSE STORAGE AREA, BIKE STORAGE AND PARKING AT THE ENTRANCE; REMOVAL OF SHEDS	
P/0118/10	SUBMISSION OF DETAILS PURSUANT TO CONDITION 2 (WATER DISPOSAL AND ATTENUATION/STORAGE WORKS) OF PLANNING PERMISSION P/2158/09 DATED 06/11/2009 FOR TWO TEMPORARY SINGLE STOREY MODULAR BUILDINGS (TOTAL INTERNAL FLOOR AREA OF 297 CUBIC METRES)	Approved 31/03/2010
P/2821/12	VARIATION OF CONDITION 1 OF PLANNING PERMISSION P/2158/09 DATED 06-NOV-2009 TO ALLOW THE RETENTION FOR AN ADDITIONAL THREE YEARS OF TWO TEMPORARY SINGLE-STOREY MODULAR BUILDINGS	Granted 17/12/2012
P/2822/12	VARIATION OF CONDITION 3 OF PLANNING PERMISSION P/1678/08 DATED 24-JUL-2008 TO ALLOW THE RETENTION FOR AN ADDITIONAL THREE YEARS OF A TWO-STOREY TEMPORARY BUILDING ADJACENT TO THE EASTERLY SITE BOUNDARY	Granted 17/12/2012
P/5154/19	Change of use from Police Office (Class B1A) to a dwellinghouse (1 x 3 Bed) (Class C3)	Granted 06/02/2020

P/1752/20	Details pursuant to Conditions 3 (Secured Sheltered Cycle Storage) and 4 (Secure by Design Certificate) attached to planning permission P/5154/19 dated 6.2.20 for Change of use from Police Office (Class B1A) to a dwellinghouse (1 x 3 Bed) (Class C3)	Approved 15/07/2020
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4.0 CONSULTATION

- 4.1 A total of 119 consultation letters were sent to neighbouring properties regarding this application.
- 4.2 A site notice was posted on 20/06/2022.
- 4.3 The overall public consultation period expired on 20th June 2022 and no objections were received.
- 4.4 Statutory and non-statutory consultation
- 4.5 A summary of the consultation responses received are set out in the Table below.

Consultee and Summary of Comments
<p>Highways Authority– Request that the applicant provide a Parking Assessment and School Travel Plan and initially had concern that the development’s resulting loss of car parking for staff would result in an unacceptable overspill to surrounding roads. Request for the school to provide cycle parking in line with London Plan minimum standards. Following amendments to the scheme and the provision of replacement car parking and proposed cycle parking provision, the officer confirmed that they were satisfied with the proposal. They have however requested swept path drawings to indicate proposed parking spaces can be safely accessed. In addition, they have also requested a Construction Logistics Plan.</p> <p>Travel Planner –All requested changes to the travel plan have been made.</p>

5.0 POLICIES

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that:
- 5.2 'If regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts, the determination must be made in accordance with the Plan unless material considerations indicate otherwise.'
- 5.3 The Government has issued the National Planning Policy Framework [NPPF 2021] which sets out the Government's planning policies for England and how these should be applied, and is a material consideration in the determination of this application.
- 5.4 In this instance, the Development Plan comprises The London Plan 2021 [LP] and the Local Development Framework [LDF]. The LDF comprises The Harrow Core Strategy 2012 [CS], Harrow and Wealdstone Area Action Plan 2013 [AAP], the Development Management Policies Local Plan 2013 [DMP], the Site Allocations Local Plan [SALP] 2013 and Harrow Local Area Map 2013 [LAP]. The relevant policies are referenced within the report below and a summary within Informative 1

6.0 ASSESSMENT

6.1 The main issues are;

- Principle of the Development
- Character, Appearance and Design
- Residential Amenity
- Accessibility
- Traffic and Parking
- Development and Flood Risk
- Sustainability
- Fire Safety

6.2 Principle of Development

6.2.1 The relevant policies are:

- National Planning Policy Framework (2021)
- The London Plan (2021): S1, S3
- Harrow Core Strategy 2012: CS1.Z
- Harrow Development Management Policies Local Plan (2013): DM46

6.2.2 Policy S1 C of the London Plan notes that development proposals that provide high quality, inclusive social infrastructure that addresses a local or strategic need and supports service delivery strategies should be supported.

6.2.3 Policy S3 of the London Plan relates to 'Education and childcare facilities'. Part B of the policy notes that development proposals for education and child care facilities should:

- 1) Locate facilities in areas of identified need.
- 2) locate facilities in accessible locations, with good public transport accessibility and access by walking and cycling
- 3) locate entrances and playgrounds away from busy roads, with traffic calming at entrances
- 4) link to existing footpath and cycle networks to create healthy routes to schools, and other education and childcare facilities, to enable all children to travel actively to school (walk, cycle or travel by public transport)
- 5) maximise the extended or multiple use of educational facilities for community or recreational use, through appropriate design measures
- 6) encourage the shared use of services between schools, colleges, universities, sports providers, and community facilities, and between early years and health and social care providers
- 7) ensure that new developments are accessible and inclusive for a range of users, including disabled people, by adopting an inclusive design approach
- 8) ensure that facilities incorporate suitable, accessible outdoor space
- 9) locate facilities next to parks or green spaces, where possible

6.2.4 Part C of Policy S3 of the London Plan notes that development proposals should ensure that there is no net loss of education or childcare facilities, unless it can be demonstrated that there is no ongoing or future need.

6.2.5 Policy CS1.Z of Harrow's Core Strategy relates to 'Required Infrastructure'. It notes that proposals for new development will be required to demonstrate adequate capacity exists or can be secured both on and off site to serve the development. The development or expansion of physical or social infrastructure will be permitted where it is needed to serve existing or proposed development, or required to meet projected future requirements. The loss of community facilities will be resisted unless adequate arrangements are in place for their replacement or the enhancement of other existing facilities.

6.2.6 Policy DM46 of the Council's Development Management Policies Local Plan relates to 'New Community, Sport and Education Facilities' Part B of the Policy notes that proposals for the provision of new community, sport, and educational facilities will be supported where:

- a) they are located within the community that they are intended to serve;
- b) subject to (a) they are safe and located in an area of good public transport accessibility or in town centres; and
- c) there would be no adverse impact on residential amenity (see Policy DM1) or highway safety.

6.2.7 The proposed development is not considered to conflict with the interests of the above planning policy. The development involves the removal of an existing modular teaching block and the erection of a replacement building elsewhere on site. The proposed building would provide an improvement in facilities with all of the class rooms having an internal floor area of 63m², this would vastly improve teaching conditions, with some of the classrooms within the existing building being far smaller. Whilst officers note that the proposed replacement building has one less classroom than the existing building, this is considered on balance to be acceptable

given that the overall size of the classrooms would proportionally be improved. It is worth adding that the proposed building would have a greater Gross Internal Floor Area than the existing building. The proposed building would also be vastly superior in terms of its aesthetics, offering a far more attractive environment for students to learn. The need for the building is considered to be justified.

6.2.8 The proposed development would serve the existing student base, the proposal is not being used to justify an expansion in the number of pupils that the school accommodates. The proposed building is located within the confines of an existing school, as such its public transport accessibility is not of significant importance, nevertheless, the development is promoting sustainable and healthy means of transport through the provision of cycle parking and through the various measures outlined within the Travel Plan. The proposed development is not considered to impact upon the safety of students, teachers, visitors, pedestrians and vehicles, furthermore it is not considered to unduly prejudice the residential amenity of neighbouring occupants, further assessment will be provided on this within the pertinent sections of this Committee Report.

6.2.9 In light of all of the above, officers raise no objection to the principle of development.

6.3 Character, Appearance and Design

6.3.1 The relevant policies are:

- National Planning Policy Framework (2021)
- The London Plan (2021): D3, D4
- Harrow Core Strategy 2012: CS1.B
- Harrow Development Management Policies Local Plan (2013):DM1, DM22,

6.3.2 Policy D3.D(1) of the London Plan states that development should in terms of form and layout, enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions. Policy D3.D(11) goes on to states that in terms of quality and character, developments should respond to the existing character of a place by identifying the special and valued features and characteristics that are unique to the locality and respect, enhance and utilise the heritage assets and architectural features that contribute towards the local character.

6.3.3 Policy D4 of the London Plan outlines how Local Planning Authorities should scrutinise development proposals to ensure that they deliver a good design, and outlines what information should be provided in achieving this.

6.3.4 Policy CS1.B of Harrow's Core Strategy notes that proposals that would harm the character of suburban areas and garden development will be resisted. All development shall respond positively to the local and historic context in terms of design, siting, density and spacing, reinforce the positive attributes of local distinctiveness whilst promoting innovative design and/or enhancing areas of poor design.

- 6.3.5 Policy DM1 of the Development Management Policies Local Plan states that all development proposals must achieve a high standard of design and layout. Proposals which fail to achieve a high standard of design and layout, or which are detrimental to local character and appearance, will be resisted.
- 6.3.6 Policy DM22 of the Development Management Policies Local Plan provides design guidance on trees and hard and soft landscaping for proposals. Part B of this policy provides an assessment framework on appropriate forms of hard and soft landscaping.
- 6.3.7 The proposed development is considered to be acceptable on character and design grounds. The existing building to be demolished is of very poor architectural merit as such there is no objection to its removal. The demolition of the building would generate much needed open space to the side of the main building, with this particular area currently appearing cramped and confined. The height, scale and massing of the proposed building would be comparable to that of other buildings within the site, it would not appear as an alien addition in context with surrounding buildings. The form and appearance of the proposed building would not be overly complex and would be appropriate for a school building, the use of a facing brick and render is consistent with other finishes applied to buildings within the school complex. The proposed building would be visible from the street scene along Merlins Avenue, however based on the fact that the height of this building would not be excessive, and it would be sited a considerable distance from this road (approximately 18.6 metres) it is not considered to appear as unduly prominent and visually intrusive when viewed along the street scene.
- 6.3.8 The proposed staff/visitor cycle shelter located to the front of the site would be visible from the street scene along Eastcote Lane but is not considered to be excessive in terms of its height and footprint, and as such it is not considered to unduly impact upon visual amenity afforded along the street scene. The other staff cycle shelter may be marginally visible from the street scene, however any harm would be de minimus. The proposed cycle shelters allocated to students would not be visible from the street scene, as such no harm would be caused to this.
- 6.3.9 The existing hardstanding currently in place below the existing building being demolished is to be retained. Whilst officers would have ideally liked to see this area allocated for soft landscaping, given that this area is limited in size, it has to provide replacement parking and cycle parking, and has a limited visibility from the site's surroundings, this is considered on balance to be acceptable.

6.4 Residential Amenity

6.4.1 The relevant policies are:

- National Planning Policy Framework (2021)
- The London Plan (2021): D3
- Harrow Development Management Policies Local Plan (2013): DM1

- 6.4.2 Policy DM1 of the Development Management Policies Local Plan seeks to ensure that "proposals that would be detrimental to the privacy and amenity of neighbouring occupiers, or that would fail to achieve satisfactory privacy and amenity for future occupiers of the development, will be resisted".
- 6.4.3 The proposed development is not considered to unduly impact upon light, outlook and visual amenity afforded to any neighbouring properties. Whilst officers note that the proposed replacement building would be visible from the rear gardens and rear facing habitable room windows of certain residential properties along Merlins Avenue, Minehead Road and Hamilton Crescent, by reason of the building's significant separation distance from these neighbouring properties, the proposed building is not considered to result in undue harm in respect to surrounding neighbouring properties. There would be a separation gap of approximately of approximately 22.1m between the proposed building and the boundary of the closest neighbouring property along Merlins Avenue, the gap would be approximately 22.3m between the building and the boundary of the closest neighbouring property along Minehead Road, and finally the gap would be approximately 37.25m between the building and the closest neighbouring property along Hamilton Crescent. The separation distances would be sufficient enough to avoid harmful impacts relating to overlooking through the proposed building's windows (both perceived and actual).
- 6.4.4 The staff and visitor cycle shelter located to the front of the site would be too distant from the private amenity areas and habitable room windows of surrounding neighbours to result in undue harm. Whilst officers note that the other staff cycle shelter would directly abut the western boundary of the site parallel to the rear gardens of No. 34 and 32 (to a lesser extent) Hamilton Crescent, there would be a slight separation gap between the shelter and the rear gardens of these neighbouring properties furthermore it should be added that No. 34 Hamilton Crescent features a rear outbuilding abutting its rear boundary, meaning that the cycle shelter would have minimal impact on this property. This cycle shelter is indicated to have a height of only 2.2 metres meaning that it would not protrude significantly above any neighbouring boundary treatment, it should be added that there is tall vegetation along this boundary which would significantly restrict views of the cycle shelter from these neighbouring properties. The cycle shelters serving students would be too distant from surrounding neighbouring properties to result in undue harm.

6.5 Accessibility

6.5.1 The relevant policies are:

- National Planning Policy Framework (2021)
- The London Plan (2021): D5
- Harrow Development Management Policies Local Plan (2013): DM2

6.5.2 London Plan policy D5 notes that development proposals should achieve the highest standards of accessible and inclusive design. They should:

- 1) be designed taking into account London's diverse population
- 2) provide high quality people focused spaces that are designed to facilitate social interaction and inclusion
- 3) be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment
- 4) be able to be entered, used and exited safely, easily and with dignity for all
- 5) be designed to incorporate safe and dignified emergency evacuation for all building users. In all developments where lifts are installed, as a minimum at least one lift per core (or more subject to capacity assessments) should be a suitably sized fire evacuation lift suitable to be used to evacuate people who require level access from the building.

6.5.3 Policy DM2 of the Development Management Policies Local Plan notes that the location, design and layout of development, and any associated improvements to public realm, transport and other infrastructure, will be required to contribute to the creation of lifetime neighbourhoods. In particular:

- b) non-residential development and change of use proposals must be accessible to all.

6.5.4 The proposed development is not considered to be in conflict with the above planning policy, as indicated within the submitted Design and Access Statement the building would be accessible to both able bodied and disabled users with it having level access and featuring a lift in addition to two stair cores.

6.6 Traffic, Parking and Refuse

6.6.1 The relevant policies are:

- National Planning Policy Framework (2021)
- The London Plan (2021): T5, T6
- Harrow Core Strategy 2012: CS1.Q, CS1.R, CS1.S
- Harrow Development Management Policies Local Plan (2013): DM42, DM43 DM44

6.6.2 London Plan maximum car parking standards are outlined within Policy T6 of the London Plan. Policy T5 outlines minimum cycle parking standards.

6.6.3 Policy T7 of the London Plan relates to deliveries servicing and construction. Part G of this policy notes that development proposals should facilitate safe, clean and efficient deliveries and servicing. Provision of adequate space for servicing, storage and deliveries should be made off-street, with on-street parking bays used only when this is not possible.

- 6.6.4 Policy DM42 of the Development Management Plan relates to the Council's parking standards. Criterion (F) of this Policy notes that proposals that would result in inappropriate on-site parking provision and those which would create significant on-street parking problems, prejudice highway safety or diminish the convenience of pedestrians and cyclists will be resisted.
- 6.6.5 Policy DM43 of the DMP relates to Transport Assessments and Travel Plans. Part A of the policy notes that proposals for major development will be required to submit a Transport Assessment for objective review by the Council. The Transport Assessment should quantify the impacts of the proposal upon public transport, the highway network, the cycle network, and upon conditions for pedestrians. Part B of the policy notes that proposals for major development will be required to satisfactorily mitigate the impacts identified in the Transport Assessment and any others arising from the Council's assessment of it. Mitigation measures will be required to contribute to the desirability of achieving modal shift away from private car use and should include the preparation and implementation of Travel Plans. Part C notes that proposals that fail to satisfactorily mitigate the transport impacts of development will be resisted. Whilst the proposal is not a major development, given that it relates to the development of a school which is already expected to make a shift towards healthier and more sustainable modes of transport outside of this application, Policy DM43 is relevant.
- 6.6.6 Policy DM44 of the Council's Development Management Plan relates to servicing. Within Part C, it is emphasised that proposals that will be detrimental to safety, traffic flow or the amenity of neighbouring occupiers will be resisted.
- 6.6.7 There has been extensive communication between the applicant and the Council's Highways Authority, who in spite of their initial concerns in regards to overspill car parking to surrounding roads has confirmed that they do not raise objection to the development following amendments made to proposed car parking and through the provision of cycle parking. The proposal would only result in a net loss of 2 no car parking spaces, the development's provision of a high quantum of cycle parking and other initiatives for sustainable travel outlined within the School Travel Plan are considered to make up for the loss. In order to make sure that the proposed replacement parking spaces can all be accessed, the Council's Transport Officer has requested swept path drawings. These have been requested and will be included and will be reported in the addendum to this Committee Report. Furthermore, given that the proposal requires extensive demolition and construction works, officers have suggested a condition requiring the applicant to provide a Construction Logistics Plan. The application site currently provides 20 existing cycle parking spaces on site, the applicant is proposing an additional 160 cycle parking spaces. The London Plan minimum cycle parking standards set out that schools will be required to provide a minimum of 1 no long stay space for 8 full time staff and 1 no long stay spaces for every 8 students. 1 no short stay space is expected for every 100 students, The Council's Transport Officer has reviewed the total number of cycle parking spaces provided for the development and has raised no objection.

6.6.8 The applicant has submitted a School Travel Plan alongside their application which has been closely scrutinised by the Council's Travel Planner and has been designed in line with their requests and recommended commitments. Based on the fact that the Travel Plan has been designed stringently in accordance with comments provided by the Council's Travel Planner, it is considered to be acceptable. As previously set out within this Committee Report, this Travel Plan is not solely linked to the planning application, it will be an on-going element of the operation of the school going forward.

6.7 Development and Flood Risk

6.7.1 The relevant policies are:

- National Planning Policy Framework (2021)
- London Plan (2021): SI 12, SI 13
- Harrow Core Strategy (2012): CS1.U, CS1.W
- Harrow Development Management Policies Local Plan (2013): DM9, DM10

6.7.2 The proposal would result in an increase in development footprint on the site and would therefore have an impact in terms of increased surface water flood risk. The applicant is not proposing to build on areas of the site that are located within Floods 3a and 3b, and the proposed building is located well away from these areas. The proposed replacement building is however located within a Critical Drainage Area, as such Sustainable urban drainage [SUDs] is encouraged. An informative is therefore attached to this effect. An informative has also been included with regard to surface and foul water connections and has advised the applicant to contact Council's Drainage Engineers to provide a drainage plan.

6.8 Sustainability

6.8.1 The relevant policies are:

- National Planning Policy Framework (2021)
- Harrow Core Strategy 2012: CS1.T
- Harrow Development Management Policies Local Plan (2013): DM12, DM14

6.8.2 Policy DM 12 of the Harrow Development Management Policies Local Plan seeks to ensure that the design and layout of development proposals are sustainable. It states that development will need to "utilise natural systems such as passive solar design and, wherever possible incorporate high performing energy retention materials"... Proposals should make provision for natural ventilation and shading to prevent internal overheating and incorporate techniques that enhance biodiversity". Policy DM14 highlights that development proposals should incorporate renewable energy technology where feasible.

6.8.3 For minor development proposals, the development plan at this point does not set out energy and sustainability targets greater than those required by Building Regulations. As these standards would be secured through other legislation, the proposal is not considered to conflict with sustainability policies in the development plan.

6.9 Fire Safety

6.9.1 The relevant policies are:

- National Planning Policy Framework (2021)
- London Plan (2021): D12

6.9.2 Policy D12 of the London Plan notes that in the interests of fire safety and to ensure the safety of all building users, all development proposals must achieve the highest standards of fire safety. The applicant has provided no detail on the development's compliance with fire safety standards as part of their submission, consequently, a condition has been applied requiring the applicant to demonstrate the development's compliance with relevant fire safety standards.

7.0 CONCLUSION AND REASONS FOR RECOMMENDING APPROVAL

7.1 The proposal would result in an improvement in the school's educational facilities whilst ensuring that the proposed building would not detract from the character and appearance of the application site, street scene and area in a wider context, and would have an acceptable impact upon the residential amenity of neighbouring properties. The replacement building would be appropriately accessible for both able-bodied and disabled users. The development's provision of car and cycle parking is acceptable, and compliance with the submitted travel plan ensures that there would be sustainable travel by students, staff and visitors to the school. The development is not considered to be susceptible to harmful flooding and is not considered to exacerbate flood risk elsewhere. Insufficient detail has been provided on the development's compliance with Secured by Design and fire safety requirements, additional detail on this can be requested and secured via conditions, a condition has also been applied requiring the applicant to submit a Construction Logistics Plan given the scale of the development and demolition works required. The Council's Transport Officer has requested swept path drawings to demonstrate that proposed replacement car parking spaces can be safely accessed, this has been requested and will be provided as part of an addendum.

7.2 Subject to conditions and the provision of swept path drawings, the proposed development would be in accordance with the National Planning Policy Framework (2021), Policies GG1, GG2, GG3, D3, D4, D5, D11, D12, D13, D14, S1, S3, SI 12, SI 13, T1, T2, T3, T4, T5, T6, T7 of the London Plan (2021), Policies CS1.B, CS1.E, CS1.T, CS1.U, CS1.W, CS1.Z, of the Harrow Core Strategy, Policies DM1, DM2, DM9, DM10, DM12, DM14, DM22, DM42, DM43, DM44, DM46 of the Harrow Development Management Policies Local Plan.

APPENDIX 1: CONDITIONS AND INFORMATIVES

Conditions

1. Timing

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON: To comply with the provisions of Section 92 of the Town and Country Planning Act 1990.

2. Approved Plans and Documents

The development hereby permitted shall be carried out in accordance with the following documents and plans: Design and Access Statement, 0655-011 (Existing Floor Plans and Elevations), 0655-012 (Proposed Floor Plans and Elevations), 0655-013 Rev B (Car Parking Strategy), Parking Assessment Dated March 2023 (Report Ref. 2204780-02A), School Travel Plan Dated March 2023 (Report Ref. 2204780-01B), BDS Cycle Shelter – 20 Space Shelter & Bike Stands (Produced by Bikedock Solutions), BDS Shelter – 40 Space Enclosure & Bike Stands (Produced by Bikedock Solutions), Email from Agent (Dated 06/03/2023).

REASON: For the avoidance of doubt and in the interests of proper planning.

3. Materials

The external surfaces of the new building hereby approved shall be constructed in accordance with the details provided within the submitted Design and Access Statement and Drawing No. 0655-012 (Proposed Floor Plans and Elevations). The external surfaces of the proposed cycle shelters shall be constructed in accordance with details provided within the following specification documents: BDS Cycle Shelter – 20 Space Shelter & Bike Stands (Produced by Bikedock Solutions) and BDS Shelter – 40 Space Enclosure & Bike Stands (Produced by Bikedock Solutions). Any variation to the approved materials within these documents shall be submitted to the Local Planning Authority for approval in writing prior to the commencement of the relevant part of the development. The development shall be completed in accordance with the approved details and thereafter be retained.

REASON: To safeguard the character and appearance of the area in accordance with Core Policy CS1.B of the Harrow Core Strategy (2012) and Policy DM1 of the Harrow Development Management Policies Local Plan (2013).

4. Glazing

No window(s)/door(s), other than those shown on the approved plans shall be installed in the flank elevation(s) of the new building hereby permitted without the prior permission in writing of the local planning authority.

REASON: To safeguard the residential amenities of neighbouring residents, in accordance with Policy DM1 of the Harrow Development Management Policies Local Plan (2013).

5. Construction Logistics Plan

No development shall take place, including any works of demolition, until a detailed demolition and construction logistics plan has first been submitted to the Local Planning Authority and agreed in writing. The plan shall detail the arrangements for:

- a) the parking of vehicles of site operatives and visitors;
- b) loading and unloading of plant and materials;
- c) storage of plant and materials used in construction the development;
- d) the erection and maintenance of security hoardings including decorative displays and facilities for public viewing;
- e) wheel washing facilities; and
- f) a scheme for recycling/disposing of waste resulting from demolition and construction works.
- g) measures for the control and reduction of dust
- h) measures for the control and reduction of noise and vibration
- i) How traffic would be managed to minimise disruption
- j) Air quality management measures

The demolition and construction of the development shall be carried out in accordance with the plan so agreed.

REASON: To ensure that measures are put in place to manage and reduce noise and vibration impacts during demolition and construction and to safeguard the amenity of neighbouring occupiers, and to ensure that the transport network impact of demolition and construction work associated with the development is managed. To ensure that measures are agreed and in place to manage and reduce dust, noise and vibration during the demolition and construction phases of the development and manage transport impacts during the demolition and construction phases of the development. This condition is a PRE-COMMENCEMENT condition as the proposed measures must be in place prior to commencement of works.

6. Parking Arrangements

Prior to the construction of the new building, the temporary car parking spaces detailed on Drawing No. 0655-013 Rev B (Car Parking Strategy) shall be made available. The proposed building shall not be first occupied until the existing teaching building identified to be demolished has been removed and the proposed car and cycle parking to be sited in place of this has been fully installed.

REASON: To ensure that there is sufficient on-site parking throughout all stages of the development.

7. Cycle Storage

Other than when in use, cycles shall be stored at all times within the designated cycle stores detailed on Drawing No. 0655-013 Rev B (Car Parking Strategy)

REASON: To enhance the appearance of the development and safeguard the character and appearance of the area.

8. Fire Safety

The new building hereby permitted shall not be first occupied until a Fire Safety Statement has been submitted to and approved in writing by the Local Planning Authority, this statement shall include details of how the development will function in terms of the following:

- 1) identify suitably positioned unobstructed outside space: a) for fire appliances to be positioned on b) appropriate for use as an evacuation assembly point
- 2) is designed to incorporate appropriate features which reduce the risk to life and the risk of serious injury in the event of a fire; including appropriate fire alarm systems and passive and active fire safety measures
- 3) is constructed in an appropriate way to minimise the risk of fire spread
- 4) provide suitable and convenient means of escape, and associated evacuation strategy for all building users
- 5) develop a robust management strategy for evacuation which is to be periodically updated and published (details of how often this management strategy is to be reviewed and published to be included), and which all building users can have confidence in
- 6) provide suitable access and equipment for firefighting which is appropriate for the size and use of the development.

The development shall be operated in accordance with the approved details in perpetuity.

REASON: To ensure that the fire safety of the proposed building is managed in a satisfactory manner and that the development contributes to fire safety in line with Policy D12A of the London Plan (2021).

9. Secure by Design

Evidence of certification of Secure by Design Accreditation for the development shall be submitted to the Local Planning Authority in writing for approval before any part of the new building is occupied or used. The development shall only be completed and operated in accordance with any approval.

REASON: In the interests of creating safer and more sustainable communities and to safeguard amenity by reducing the risk of crime and the fear of crime

INFORMATIVES:

1. Policies

The following policies are relevant to this decision:

The National Planning Policy Framework (2021)

London Plan 2021: GG1, GG2, GG3, D3, D4, D5, D11, D12, D13, D14, S1, S3, SI 12, SI 13, T1, T2, T3, T4, T5, T6, T7

The Harrow Core Strategy 2012: CS1.B, CS1.E, CS1.T, CS1.U, CS1.W, CS1.Z

Harrow Development Management Policies Local Plan 2013:

DM1, DM2, DM9, DM10, DM12, DM14, DM22, DM42, DM43, DM44, DM46

2. Considerate Contractor Code of Practice

The applicant's attention is drawn to the requirements in the Considerate Contractor Code of Practice. In the interests of minimising any adverse effects arising from building operations, the limitations on hours of working are as follows: 0800-1800 hours Monday - Friday (not including Bank Holidays) 0800-1300 hours Saturday

3. Party Wall Act:

The Party Wall etc. Act 1996 requires a building owner to notify and obtain formal agreement from adjoining owner(s) where the building owner intends to carry out building work which involves:

1. work on an existing wall shared with another property;
 2. building on the boundary with a neighbouring property;
 3. excavating near a neighbouring building,
- and that work falls within the scope of the Act.

Procedures under this Act are quite separate from the need for planning permission or building regulations approval.

"The Party Wall etc. Act 1996: explanatory booklet" is available free of charge from:

Communities and Local Government Publications, PO Box 236, Wetherby, LS23 7NB

Please quote Product code: 02 BR 00862 when ordering.

Also available for download from the Portal website:

<https://www.gov.uk/party-wall-etc-act-1996-guidance>

4. Liability For Damage to Highway

The applicant is advised to ensure that the highway is not interfered with or obstructed at any time during the execution of any works on land adjacent to a highway. The applicant is liable for any damage caused to any footway, footpath, grass verge, vehicle crossing, carriageway or highway asset. Please report any damage to nrswa@harrow.gov.uk or telephone 020 8424 1884 where assistance with the repair of the damage is available, at the applicants expense. Failure to report any damage could result in a charge being levied against the property.

5. Grant without Pre-App Advice

Statement under Article 35(2) of The Town and Country Planning (Development Management Procedures) (England) Order 2015

This decision has been taken in accordance with paragraphs 39-42 of The National Planning Policy Framework. Harrow Council has a pre-application advice service and actively encourages applicants to use this service. Please note this for future reference prior to submitting any future planning applications.

6. Surface and Foul Water Connections

The applicant is advised that the Drainage Authority in Harrow recommends the submission of a drainage plan, for their approval, indicating all surface and foul water connections and their outfall details. Please also note that separate systems are used in Harrow for surface water and foul water discharge. Please email infrastructure@harrow.gov.uk with your plans.

7. Sustainable Drainage Systems

The applicant is advised that surface water run-off should be controlled as near to its source as possible through a sustainable drainage approach to surface water management (SUDS). SUDS are an approach to managing surface water run-off which seeks to mimic natural drainage systems and retain water on or near the site as opposed to traditional drainage approaches which involve piping water off site as quickly as possible.

SUDS involve a range of techniques including soakaways, infiltration trenches, permeable pavements, grassed swales, ponds and wetlands. SUDS offer significant advantages over conventional piped drainage systems in reducing flood risk by attenuating the rate and quantity of surface water run-off from a site, promoting groundwater recharge, and improving water quality and amenity.

Where the intention is to use soak ways they should be shown to work through an appropriate assessment carried out under Building Research Establishment (BRE) Digest 365.

Support for the SUDS approach to managing surface water run-off is set out in the National Planning Policy Framework (NPPF) and its accompanying technical

guidance, as well as the London Plan. Specifically, the NPPF (2019) gives priority to the use of sustainable drainage systems in the management of residual flood risk and the technical guidance confirms that the use of such systems is a policy aim in all flood zones. Policy 5.13 of the London Plan (2016) requires development to utilise sustainable drainage systems unless there are practical reasons for not doing so. Sustainable drainage systems cover the whole range of sustainable approaches to surface drainage management. They are designed to control surface water run-off close to where it falls and mimic natural drainage as closely as possible. Therefore, almost any development should be able to include a sustainable drainage scheme based on these principles.

The applicant can contact Harrow Drainage Section for further information

8. Thames Water

The applicant can contact Thames Water developer services by email: developer.services@thameswater.co.uk or by phone: 0800 009 3921 or on Thames Water website www.developerservices.co.uk for drainage connections consent.

Checked

 <p>Orla Murphy Head of Development Management</p> <p>6th April 2023</p>	 <p>Viv Evans Chief Planning Officer</p> <p>6th April 2023</p>
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APPENDIX 2: SITE PLAN



Site Location Plan @ 1:1250



Scale in metres

APPENDIX 3: SITE PHOTOS

Existing Building to be Removed:



Existing Building to be Removed:



Existing Building to be Removed:



Existing Building to be Removed:



View of Rear Car Park:



View of Rear Car Park:



View of Rear Car Park:



View of Rear Car Park:



View of Rear Car Park:



View of Rear Car Park:



View of Rear Car Park:



View of Rear Car Park:



View to Merlins Avenue Rear to the Rear of the Site:



View Along Merlins Avenue:

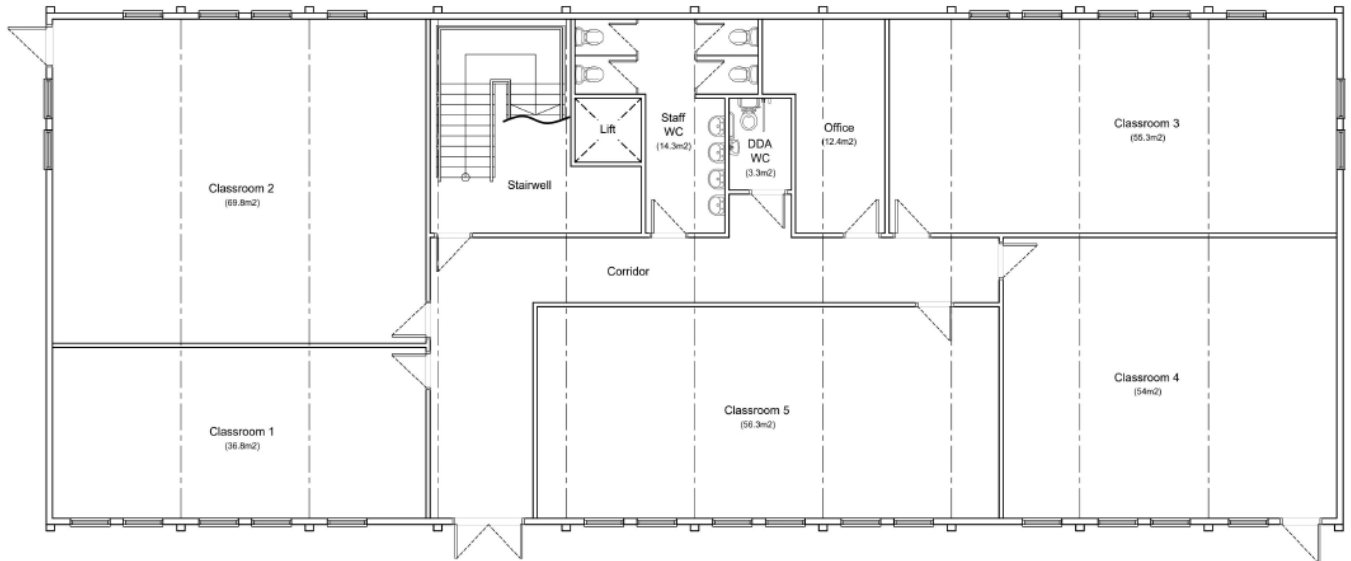


View of Front Car Park:

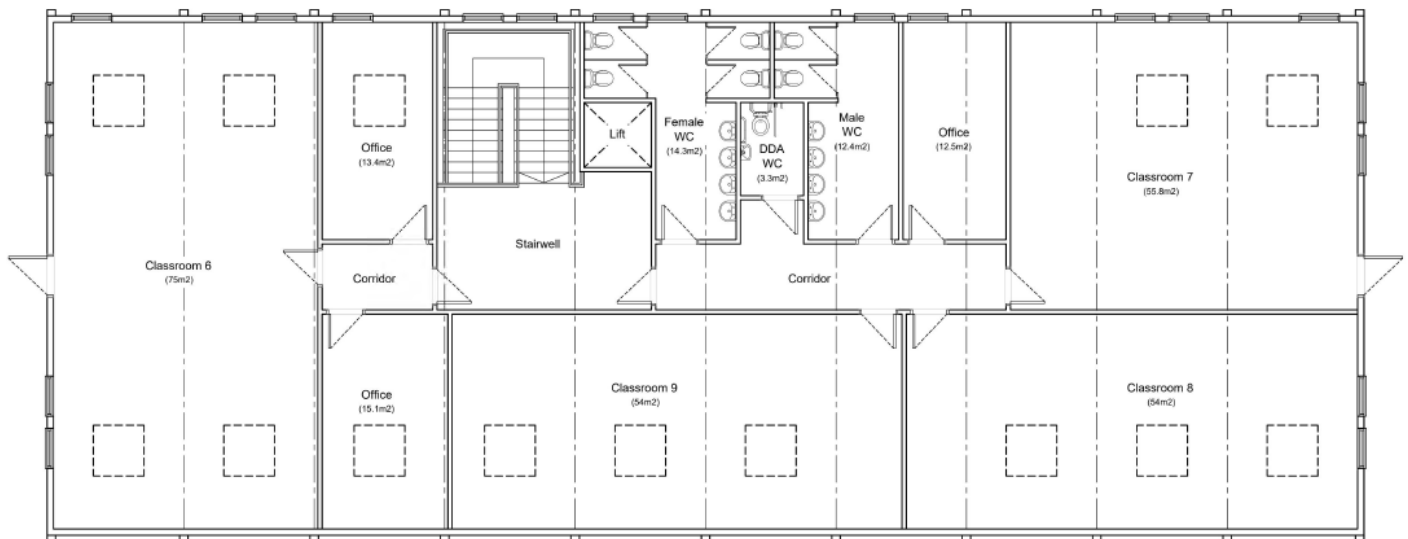


APPENDIX 4: PLANS AND ELEVATIONS

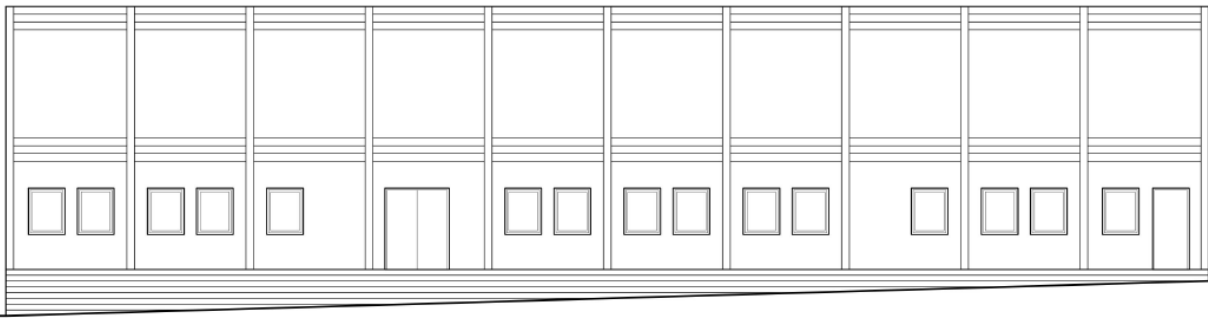
Existing Ground Floor Plan:



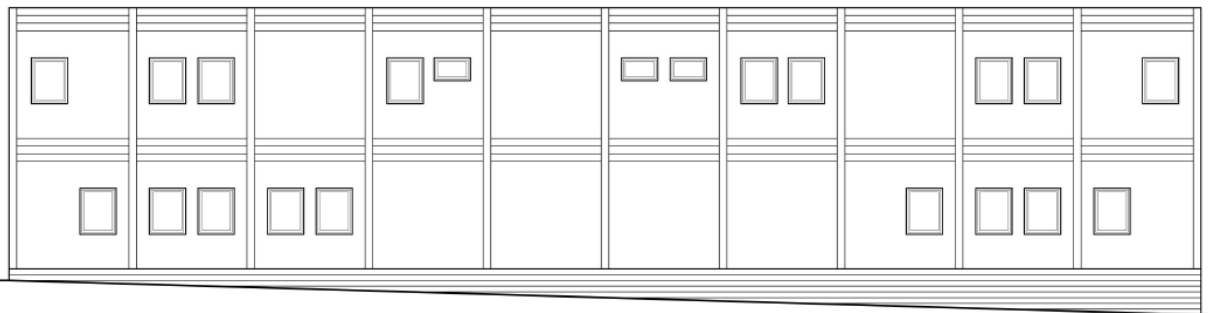
Existing First Floor Plan:



Existing Front and Rear Elevations:

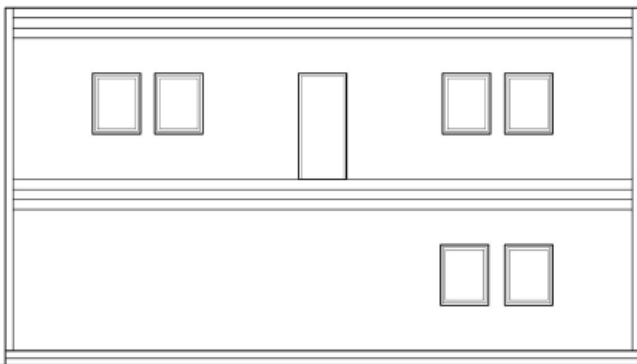


Existing front elevation @ 1:100

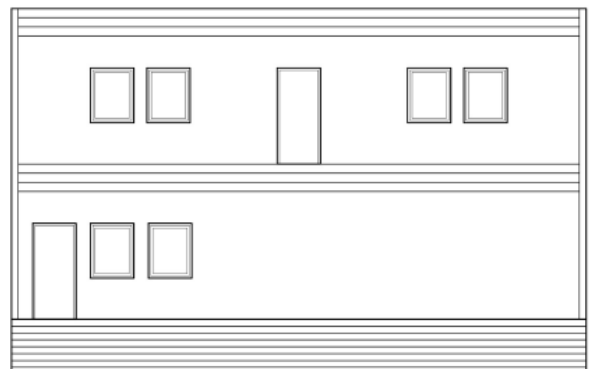


Existing rear elevation

Existing Side Elevations:



Existing side elevation



Existing side elevation

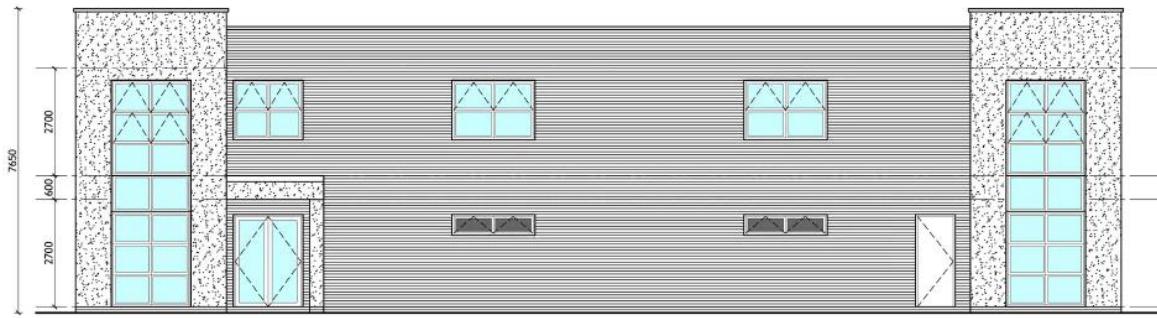
Proposed Ground Floor Plan:



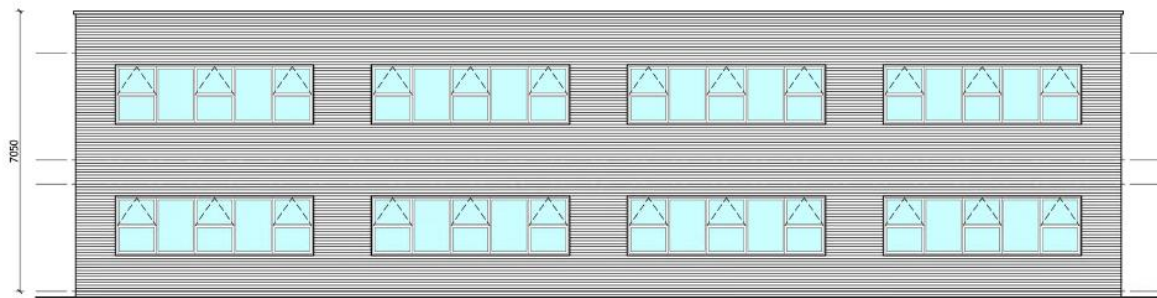
Proposed First Floor Plan:



Proposed Front and Rear Elevations:

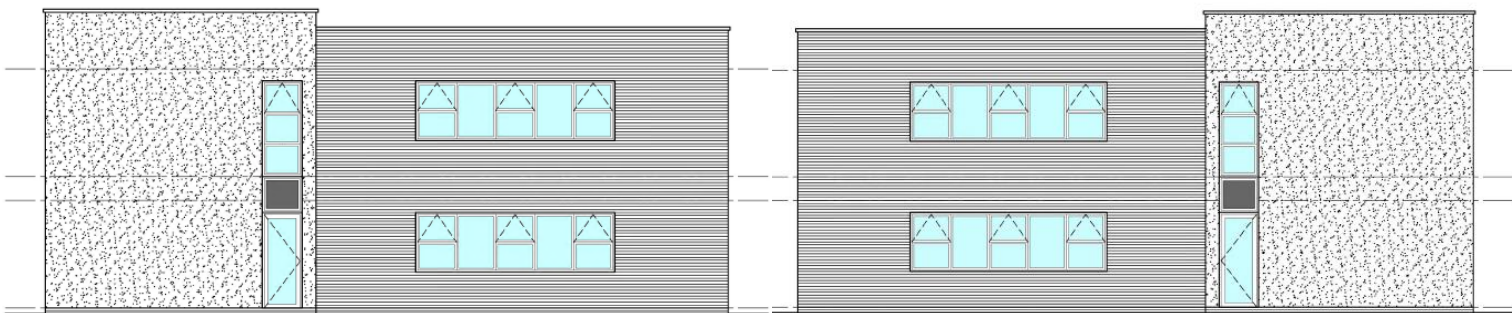


Proposed front elevation @ 1:100



Proposed rear elevation

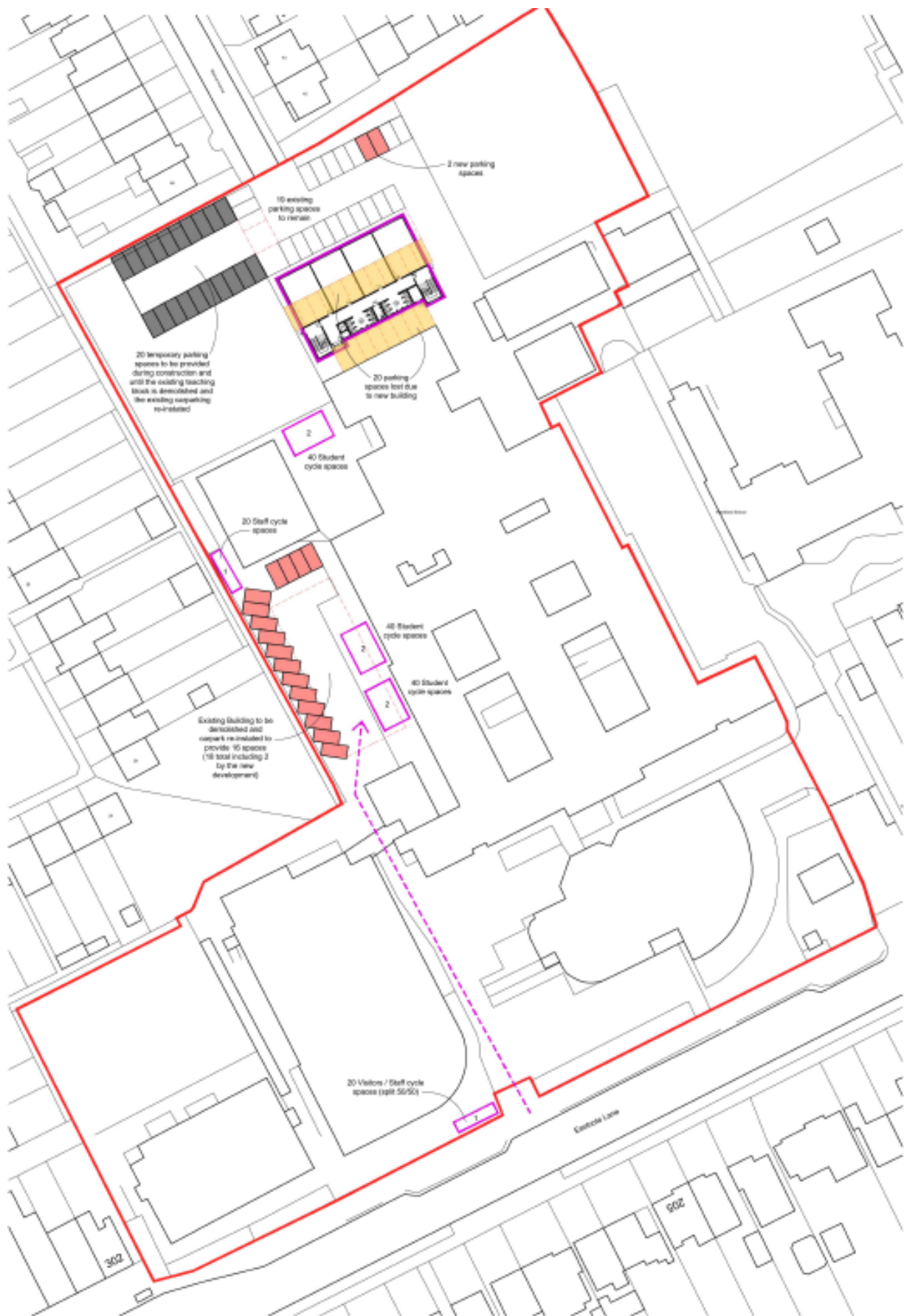
Proposed Side Elevations:



Proposed side elevation

Proposed side elevation

Proposed Site Plan:



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